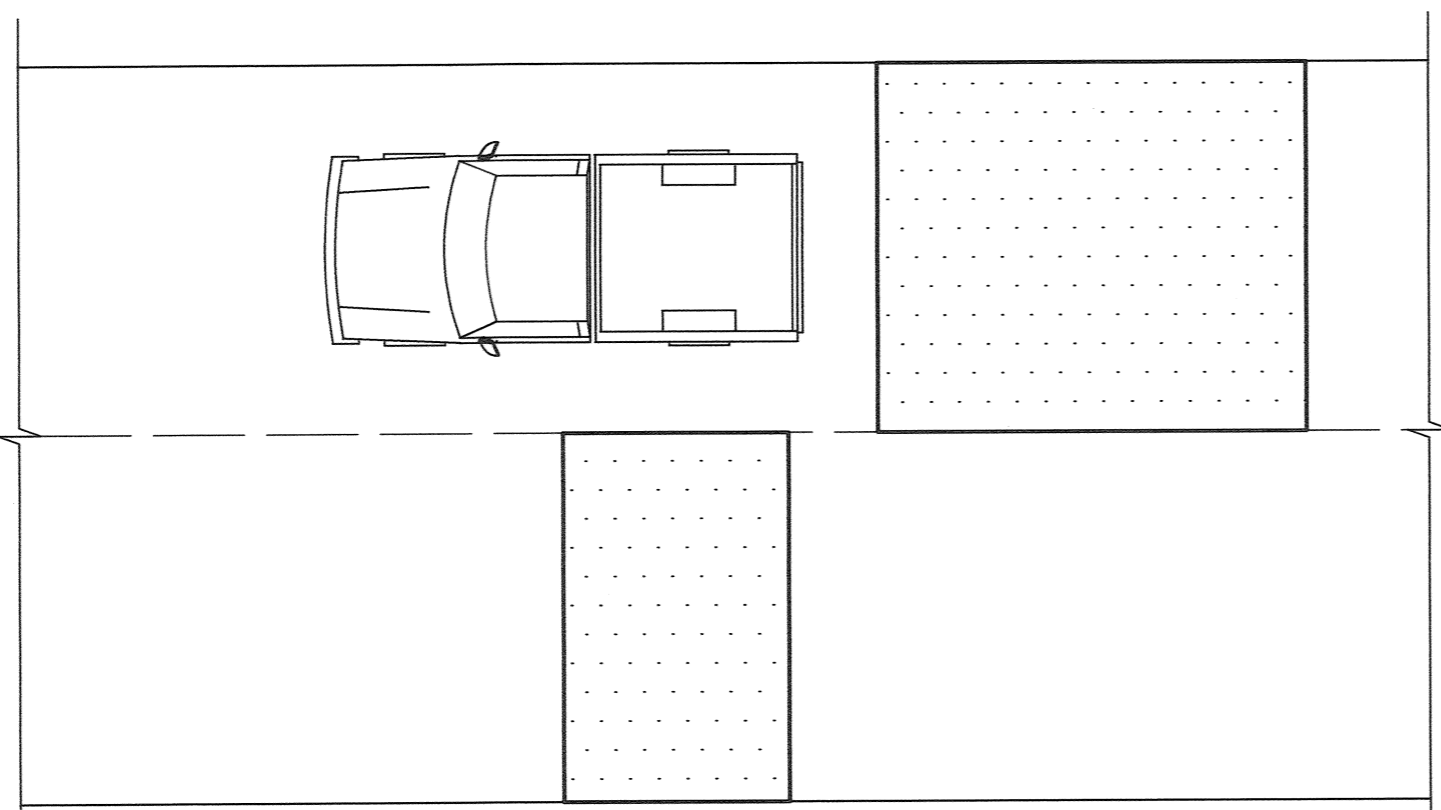
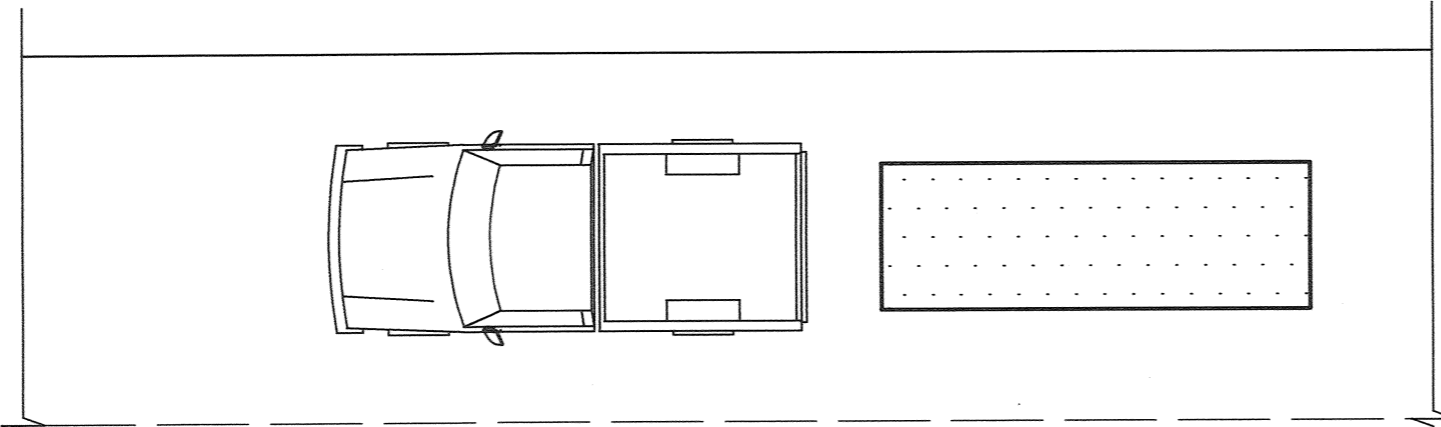


REVISIONS			
NO.	DATE	BY	REMARKS
△	4/13/21		ADDED NOTE 8



ACCEPTABLE



NOT ACCEPTABLE

NOTES

- EXISTING PAVEMENTS SHALL BE REMOVED TO CLEAN, STRAIGHT LINES PARALLEL AND PERPENDICULAR TO THE FLOW OF TRAFFIC.
 - DO NOT CONSTRUCT PATCHES WITH ANGLED SIDES AND/OR IRREGULAR SHAPES.
 - ALL REPAIRS SHALL BE FULL LANE WIDTH.
 - FOR PATCHES IN ASPHALT, A TACK COAT SHALL BE APPLIED TO ALL EDGES OF THE EXISTING ASPHALT BEFORE PLACING THE NEW PAVEMENT.
 - AFTER PLACING THE NEW ASPHALT, ALL SEAMS (JOINTS) BETWEEN THE NEW AND EXISTING PAVEMENTS SHALL BE SEALED WITH AN ASPHALT TACK COAT OR RUBBERIZED CRACK SEAL MATERIAL.
 - TRAVERSE PATCHES SHALL BE OVERLAID ACROSS THE ENTIRE STREET WIDTH FOR A DISTANCE OF TWO (2) FEET MINIMUM ON ALL SIDES OF THE TRENCH.
 - THE EDGES OF PATCHES PARALLEL TO THE DIRECTION OF TRAFFIC SHALL BE LIMITED TO THE BOUNDARIES OF LANES OR TO THE CENTERLINE OF TRAVEL LANES.
- △ 8. ALL CUT ASPHALT EDGES TO BE TREATED WITH A TACK COAT

NOT TO SCALE

**CITY OF MEMPHIS
DIVISION OF ENGINEERING**

DESIGN STANDARD
FOR
**PAVEMENT REPAIR
WHEEL PATH DETAILS**

<i>Chae Chew</i>	12.11.19
<small>CIVIL DESIGN ENGINEER</small>	<small>DATE</small>
<i>WZ</i> 12/16/19	
<small>CITY ENGINEER</small>	<small>DATE</small>