

| REVISIONS |         |    |              |
|-----------|---------|----|--------------|
| NO.       | DATE    | BY | REMARKS      |
| Δ         | 4/13/21 |    | ADDED NOTE 2 |
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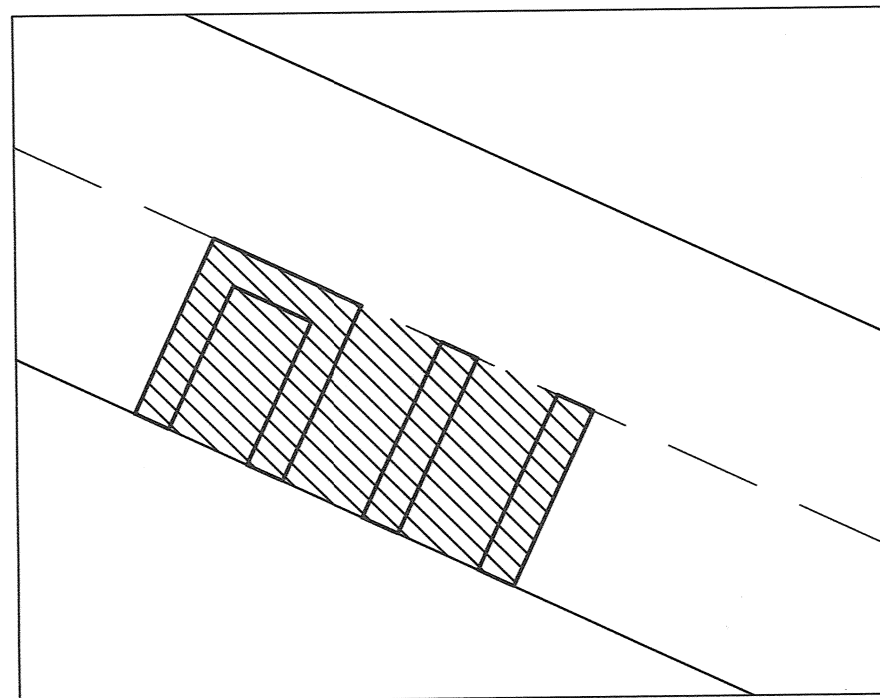
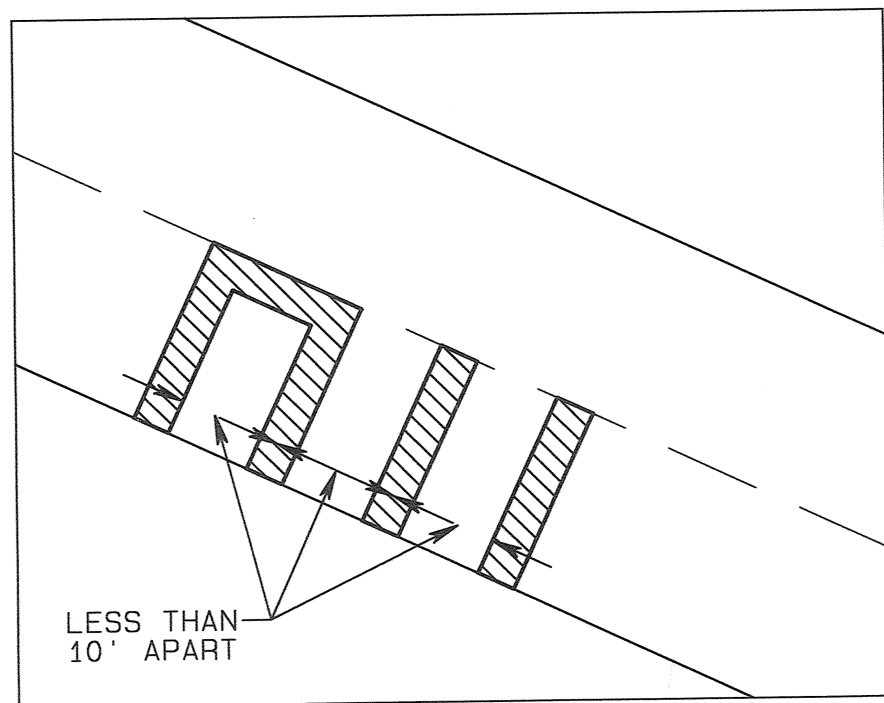
NOTES

1. IN THE CASE OF A SERIES OF PATCHES OR PATCHES FOR SERVICE LINES OFF A MAIN TRENCH, REPAIR THE PAVEMENT OVER THE PATCHES BY GRINDING AND OVERLAY WHEN THE SPACING BETWEEN THE PATCHES IS LESS THAN 10 FEET. IN CASES WHERE THE EXISTING PAVEMENT IS IN POOR CONDITION( IN THE STRATEGIC PAVING PLAN) AND MAY REQUIRE OVERLAY WITHIN THE NEXT FEW YEARS, THIS REQUIREMENT MAY BE MODIFIED OR WAIVED BY THE CITY ENGINEER.

Δ 2. ALL CUT ASPHALT EDGES TO BE TREATED WITH A TACK COAT

NOT ACCEPTABLE

ACCEPTABLE



NOT TO SCALE

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|---|------------------|
| CITY OF MEMPHIS<br>DIVISION OF ENGINEERING                  |                  |
| DESIGN STANDARD<br>FOR<br>PAVEMENT REPAIR PATCHES IN SERIES |                  |
| <i>Chae Chae</i><br>CIVIL DESIGN ENGINEER                   | 12.11.19<br>DATE |
| <i>WZ</i><br>CITY ENGINEER                                  | 12/11/19<br>DATE |